
SERVICE BULLETIN

MAINTENANCE OF WAY EQUIPMENT

DATE: 04-2020

BULLETIN NO: 20-004

TITLE: Transmission Torque Lug Fastener Upgrade

RATING:

<input type="checkbox"/>	DIRECTIVE (Action Is Required)	<input checked="" type="checkbox"/>	ALERT (Potential Problem)
<input type="checkbox"/>	INFORMATION (Action Is Optional)	<input type="checkbox"/>	PRODUCT IMPROVEMENT (Enhance Product)

PRODUCT SERIES / MODEL: MARK IV Production / Switch Tampers with Durst or Cushman Drive Transmissions

SERIAL NO: N/A

SUMMARY: Harsco Rail has become aware that the lock nuts on the bolts that secure the transmission torque link have the potential of loosening until they fall off. When this happens, the torque link mounting bolts can work out of the torque link mounting resulting in the transmission torque link disengaging from the machine frame. Follow the instructions in this Service Bulletin to replace the existing fasteners with new fasteners included in Torque Link Bolts Conversion Kit #5110548.

OPERATIONAL IMPACT: If the bolts on the transmission torque link loosen and then work out of the torque link mounting during operation or track travel, the transmission may pivot down and strike the railroad cross ties which may cause damage to the machine and track structure, machine derailment and possible personal injury to the operator.

ACTION: Follow the instructions in this Service Bulletin to inspect the transmission torque link bolts and replace the existing fasteners with new fasteners included in Harsco Rail Conversion Kit #5110548. Order one Torque Link Bolts Conversion Kit #5110548 for each transmission torque link mounting that is being reworked. For a MARK IV, two (2) total kits would be required per machine.

CONTACT: Harsco Rail Service Department
(803) 822-7546

Harsco Rail Parts Department
(800) 800-6410

SAFETY INFORMATION



- **FOLLOW APPLICABLE RAILROAD LOCKOUT - TAGOUT PROCEDURES AND MACHINE ENERGY CONTROL PROCEDURE TO DISABLE ENERGY SOURCES WHEN PERFORMING MAINTENANCE, MAKING ADJUSTMENTS OR REPAIRS TO THE MACHINE. FAILURE TO HEED THIS WARNING COULD RESULT IN SEVERE BODILY INJURY.**

Before Replacing Transmission Torque Link Bolts

1. Make sure all machine work components are in the stored position and locks are engaged.
2. Make sure the machine is shut down, parking brakes are applied and battery switch is turned off. Chock the machine wheels.
3. Inspect the two bolts that secure the torque links to the frame lug and transmission lug. If the bolts do not have cotter pins on the lock nut ends, the bolts will need to be replaced.
4. Order one Torque Link Bolts Conversion Kit #5110548 for each transmission torque link mounting that is being reworked. For a MARK IV, two (2) total kits would be required per machine.

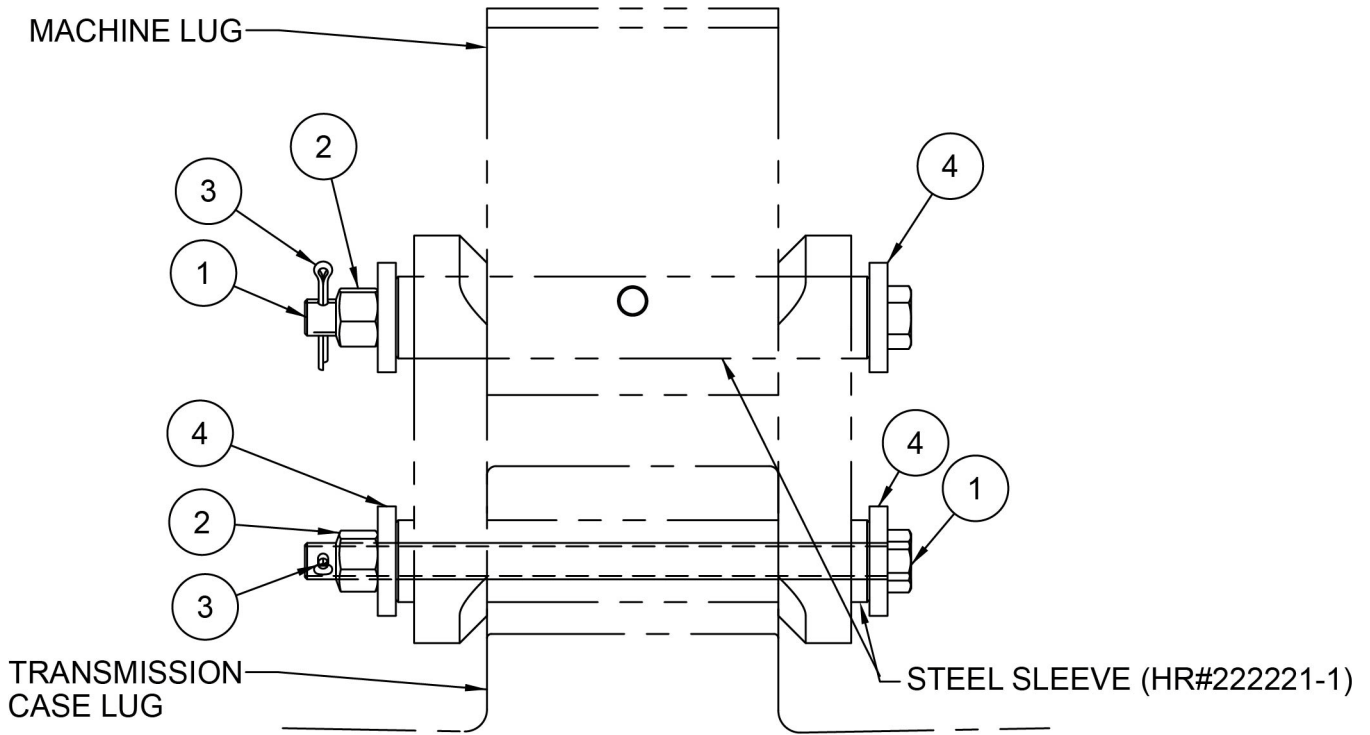
ITEM	PART NO	DESCRIPTION	QTY
	5110548	TORQUE LINK BOLTS CONVERSION KIT	1
1	5108737	Torque Link Bolt, 1/2"-13 x 8"	2
2	252275-820	Locknut, 1/2"-20 Grade 8	2
3	F001182	Cotter Pin, 1/8 X 1-1/4"	2
4	MT223-B	Washer	4

Replacing Transmission Torque Link Bolts - See Figure 1

1. Secure the front of the transmission using jack stands, blocks, etc.
2. Remove items (1), (2) and (4) and discard.
3. Install new cap screw (1), new hex lock nut (2), new washers (4) and existing steel sleeve, Harsco Rail #222221-1.
4. Secure hex lock nut (2) and install cotter pin (3). Bend both legs of the cotter pin to secure it in place.
5. Remove items (1) and (2) and discard.
6. Install new cap screw (1), new hex lock nut (2) and new washers (4).
7. Secure hex lock nut (2) and install cotter pin (3). Bend both legs of the cotter pin to secure it in place.

Replacing Transmission Torque Link Bolts

FIGURE-1
TORQUE LINK ASSEMBLY



© 2020 HARSCO CORPORATION, ALL RIGHTS RESERVED

306 West 4th Street
Fairmont, MN
56031-1837
Tel: (507) 235-7376
Fax: (507) 235-7372

2401 Edmund Road, Box 20
Cayce-West Columbia, SC
29171-0020
Tel: (803) 822-9160
Fax: (803) 822-7403

200 South Jackson Road
Ludington, MI
49431
Tel: (231) 843-3431
Fax: (231) 843-1644

Printed In USA